

**Winfield Town Centre
Streetscape Concept and Trail System
Plan**

(Vernon Creek Corridor)

URBAN SYSTEMS

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Table of Contents

	<i>Page</i>
1.0 Introduction	1
2.0 Main Street Road Alignment	2
3.0 Main Street Character	3
4.0 Pollard Road Extension	6
5.0 Streetscape Features.....	7
6.0 Vernon Creek Trail System	14
6.1 Extent and Function	14
6.2 Trails	15
6.3 Facilities.....	15
6.4 Implementation	16
7.0 Cost Estimates.....	17

This report is intended to summarize the direction for the Winfield Main Street Streetscape improvements, and define the schematic drawing that contains the various streetscape elements. The report also presents a plan for a Trail System along Vernon Creek, which together form an integral part of the Winfield Town Centre.

The *Streetscape Concept* addresses the beautification of Main Street as the core or spine of the future downtown area. Besides providing some direction to the function of Main Street as a key roadway to accommodate commercial area traffic, the *Streetscape Concept* also addresses the “pedestrianization” of Main Street. As the street develops with new land use, Main Street itself will become an identifiable feature for Lake Country.

Enhancing the streetscape will add to its identity as a focal point and hopefully help draw new businesses, local shoppers and tourists.

The preparation of the *Streetscape Concept* included input from various committees, including PARC, Public Art and the TPC along with the District of Lake Country staff and Council members. Preliminary discussions about roadway standards, parking, public art, streetscape furniture and other design elements were held before a concept was prepared and presented to the Winfield Town Centre Committee. The Streetscape treatment contained within this report represents consensus of the Committee; nevertheless, it does not preclude changes from being made at the detailed design/engineering phase for construction.

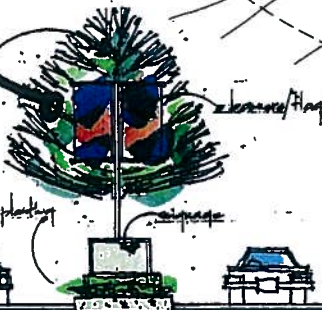
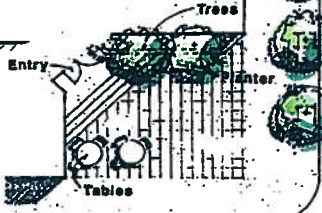
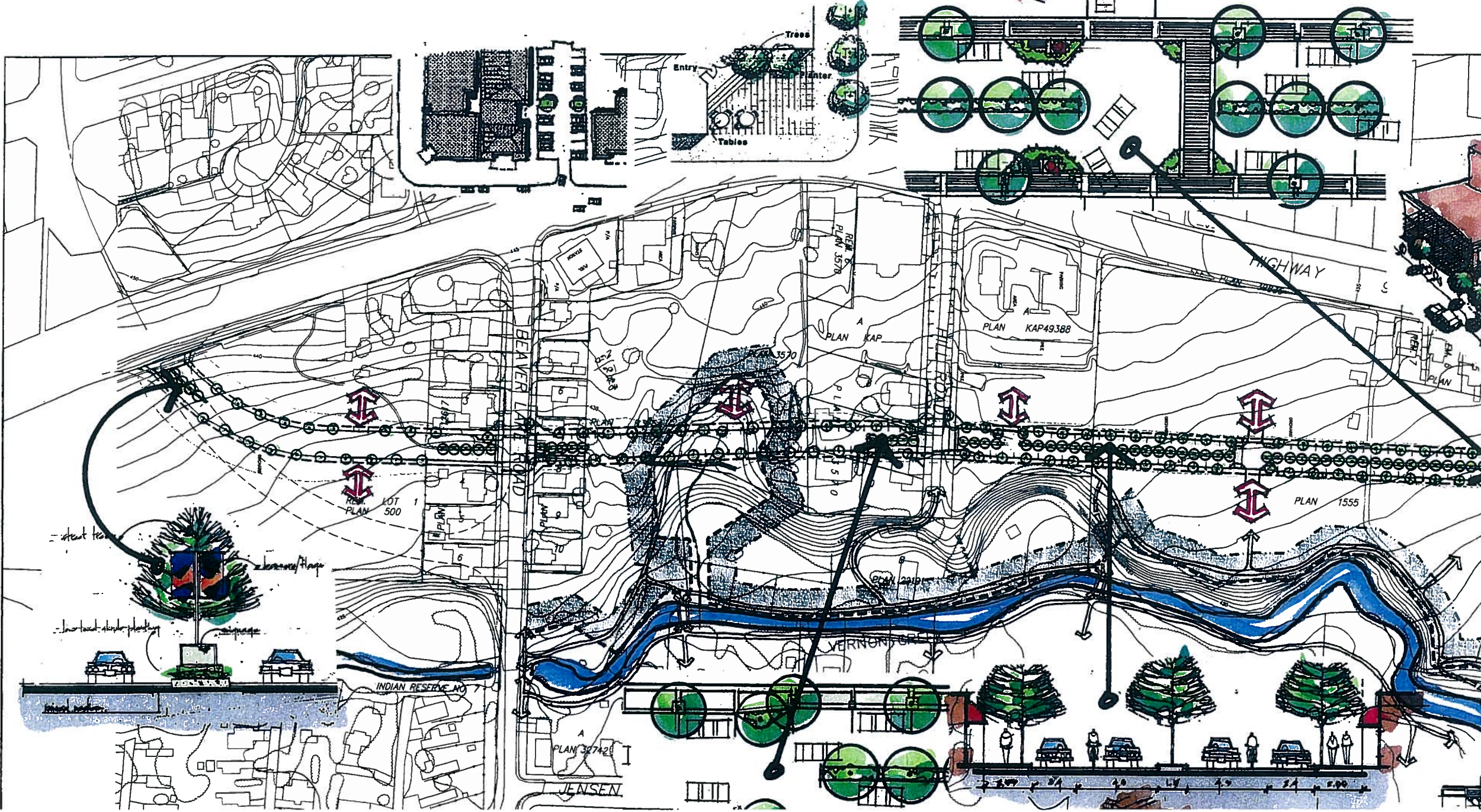
The Vernon Creek Corridor Trail system addresses a two-tiered pathway along Vernon Creek with connections to Main Street and to the east side of the creek. The main goal of the Trail system is to link all corners of the Town Centre and promote pedestrian movement along a key natural amenity area. Although the Trail system can be managed to allow for bicycles on certain parts, the system is largely intended for walking, hiking and natural area interpretation.

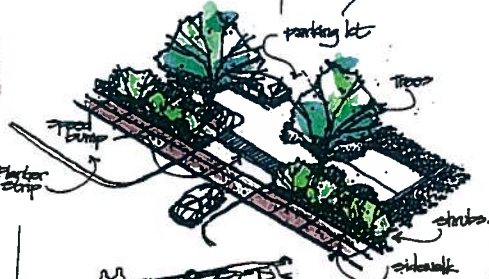
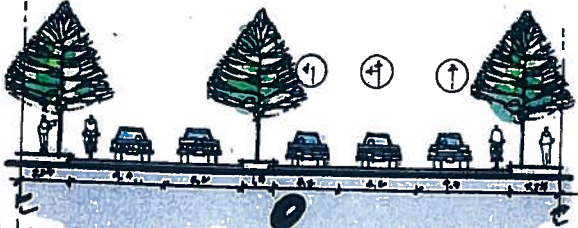
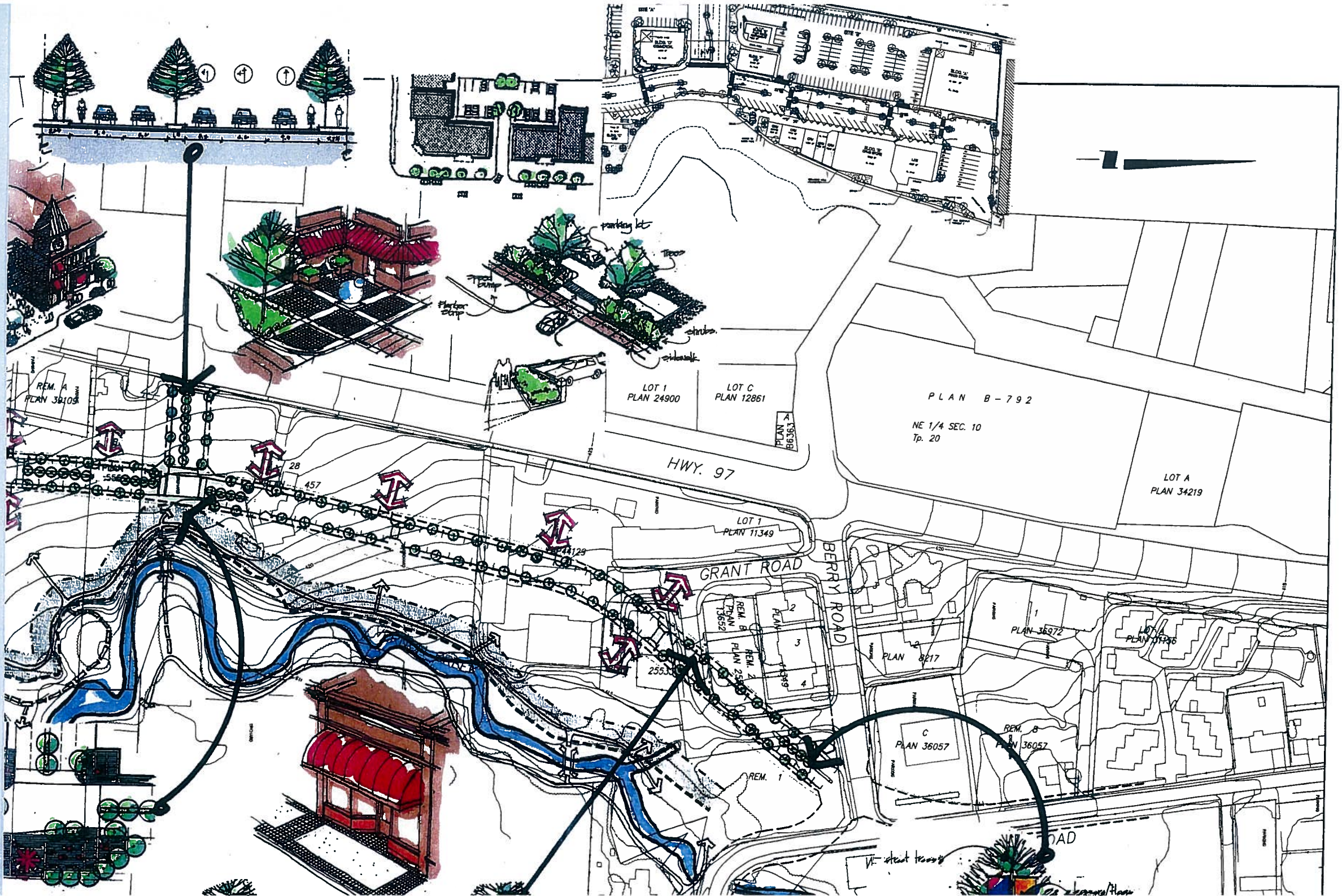
The *Streetscape Concept* proposed in this report is based on the alignment as depicted in Figure 1. The chosen alignment for Main Street runs between Highway 97 and Vernon Creek starting at a point where it intersects with Highway 97 at the south end and ends where it will intersect with Berry Road and ultimately tie-in to Woods Lake Bottom Road at the north end.

Intersecting roads that may also receive streetscape improvements in the future include Beaver Lake Road, Hill Road, Pollard Road (extension east of Highway 97) and Berry Road. This report only addresses Pollard Road as a main entry to the Town Centre, and the only other new road along with Main Street.

The Traffic Study for the Winfield Town Centre presents recommendations for access management along Main Street. Consolidated access points are strongly encouraged so that single driveways are used rather than having two or more driveways located side-by-side. These access points will influence the streetscape pattern as well turning movements off, and on to, Main Street.

The actual alignment is constrained at certain locations, however some flexibility may be permitted especially if it does not compromise safety. Please see attached Design Brief.





REM. A
PLAN 38109

LOT 1
PLAN 24900

LOT C
PLAN 12861

PLAN B-792

NE 1/4 SEC. 10
Tp. 20

LOT A
PLAN 34219

HWY. 97

LOT 1
PLAN 11349

GRANT ROAD

BERRY ROAD

PLAN 13652

PLAN 13652

PLAN 25399

PLAN 8217

PLAN 36972

PLAN 31936

PLAN 36057

PLAN 36057

REM. 1

ROAD

The Main Street character has been incorporated into its functional design and aesthetic appeal. From an aesthetic perspective, the goal has been to make the street inviting and pedestrian-friendly. The street's functional design includes a 50 km/hour design speed and a 20 meter right-of-way. The Main Street roadway incorporates a 4 lane urban cross-section, with one travel lane and one parking lane per side. Sidewalks, turning lanes and a centre median are also included along various sectors of the alignment.

The Pollard Road extension between the highway and Main Street is expected to be a main access into the Town Centre area. This roadway will require a 25 meter right-of-way to accommodate turning lanes onto and off the highway. A centre median and sidewalk/boulevard is also included.

FIVE SECTORS

The *Streetscape Concept* is based on five sectors of varying treatment:

1. Highway 97 to Beaver Lake Road

South entry to Town Centre includes a landscaped centre median treatment as an entry feature near Highway 97, and a small landscaped centre median just south of Beaver Lake Road. Median treatment along the remainder of the sector is not suggested in this proposal.

The entry feature at this location should announce arrival to the Town Centre with quality signage and banners or flags that complement the remaining streetscape.

2. Beaver Lake Road to South of Hill Road

This sector includes the standard cross-section with short stretches of median treatment just north of Beaver Lake Road and just south of Hill Road.

3. Hill Road up to and Including Pollard Road Extension

This sector is deemed to be the hub of the Town Centre and deserves some special treatment along Main Street. With the exception of locations where driveway access may impede a continuous median, this sector will contain a formal planted centre median (approximately 1.5 meters). The centre median of trees will be complemented with trees planted in the side boulevards/sidewalks. This sector also includes special sidewalk treatment of concrete pavers (approximately 2.5 meters wide).

Curb flares are suggested at both intersections and at mid-block crossings of Main Street. This will cause the carriage way of the road to be pinched to facilitate pedestrian movement and encourage traffic calming. The curb flare design should allow for enhanced landscaping/shrubs, seating areas and public art.

4. Just North Of Pollard Road Extension to Edge Of Shopping Centre Site

Streetscape treatment for this sector of Main Street reflects the intended land uses between Pollard Road and the existing Shopping Centre. It is suggested that full median treatment is not necessary in this location. A short stretch of planted median is appropriate just north of Pollard Road. Curb flares and mid-block crosswalks may be introduced at or near consolidated driveways. Sidewalk treatment will be concrete with paver block bands with an overall width of 3.3m.

5. Winfield Shopping Centre Site to Berry Road

This sector is another entry point for the Town Centre. Trees in a centre median will be included as a entry feature to be located just south of Berry Road. As in the entry feature to the south, landscaping should be complemented by signage and other elements to announce arrival into the Town Centre area. Sidewalk treatment will be concrete with paver block bands, and an overall width of up to 3.3 meters wherever there is no median.

PARKING

On-street parking is recommended on both sides of Main Street, along Hill Road and Berry Road. Since Pollard Road will need to accommodate extra travel lanes, on-street parking will not be permitted along this roadway. Parking stall dimensions will be 2.4 meters by 6.6 meters. Angle parking on Main Street is not recommended unless the sidewalk width can be compromised by the District.

BUILDING SETBACK

Another design element that will influence the Streetscape character is the front yard setback requirements along the property line. Zoning bylaw provisions have been amended to allow for a minimum 1 meter setback. This provision along with architectural design and landscape guidelines will help encourage wider sidewalks and greater amenity area along the street.

THEMATIC DESIGN

The Public Art Advisory Committee (PAAC), submitted a brief full of ideas to incorporate a theme into the Town Centre. It was acknowledged that a prescriptive approach to applying a design theme would not be appropriate especially as it would apply to building design or architectural control. The intent of their submission was more to address aesthetics, consistency of streetscape treatment and incorporation of public art as an appealing feature for the Town Centre.

The PAAC suggested "The Fruit Industry" as a central theme to help unify and characterize the Town Centre. This theme could be incorporated in many urban design elements, on public or private property. These could range from all of the many street furniture items, to decorative art on manhole covers, tree grates, and retaining walls as well as artworks. It could also be part of permanent interpretive displays along Main Street or along the Vernon Creek Corridor.

Future architectural design guidelines should also consider incorporating elements of the theme in facades, landscaping and signage.

As an initial step, the District of Lake Country should consider the theme for the Town Centre signage planned at key entry points or at civic nodes. The naming of the main street may also reflect this central theme.

The streetscape improvements for the Pollard Road extension call for a planted median along its entire length and an amenity feature at its terminus. The intent of this focal point will be to present a visual cue when entering the Town Centre off Highway 97. This small plaza area could include a significant public art feature and pads for future rotating art exhibits, interpretive displays, seating and enhanced landscape treatment. The site could also serve as a staging area for (or connection to) the trail system along Vernon Creek. The area at the end of Pollard Road may be slightly constrained by the 15 meter leave strip for Vernon Creek, but it should not preclude a strong visual statement from being made at this key location. This would imply that part of the amenity area may need to shift slightly to the south or slightly to the north.

(Note: Costs for this amenity feature have not been estimated)

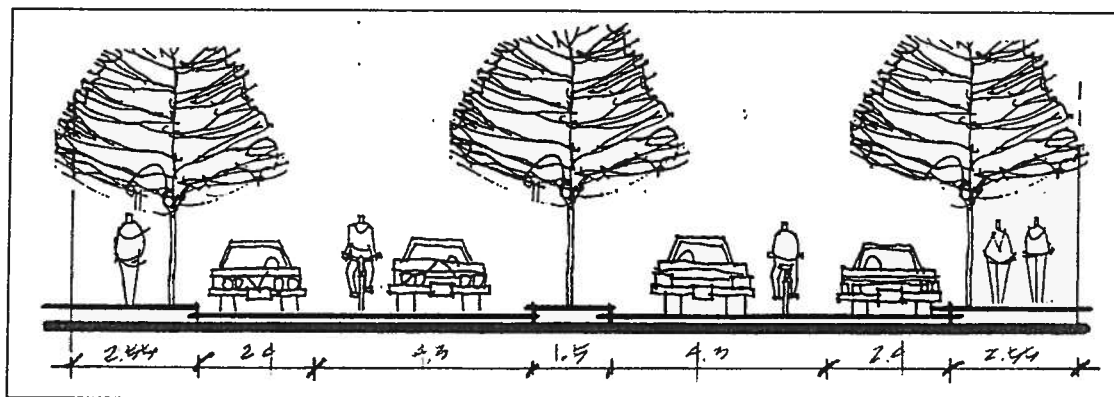
Centre Median

The centre median is the feature landscape element of the Town Centre area.

This element alone will serve to create a unique and memorable streetscape character to the Town Centre area and enrich both the pedestrian and vehicular experience of the space.

The median is typically 1.5 meters in width and provides the opportunity for street tree planting and landscape treatment in the middle of the carriageway. The median is envisioned as a regular pattern of single species street tree, planted at approx. 7 meters on centre. This spacing complements the sidewalk street tree planting layout and will eventually provide a continuous tree canopy in the central Main Street area.

The ends of the median would be ideally suited for ornamental shrub massing or annual floral displays, with the dominant centre median landscape treatment being grass or groundcovers.



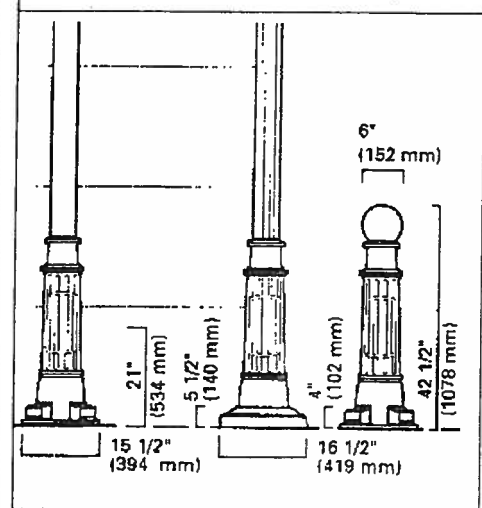
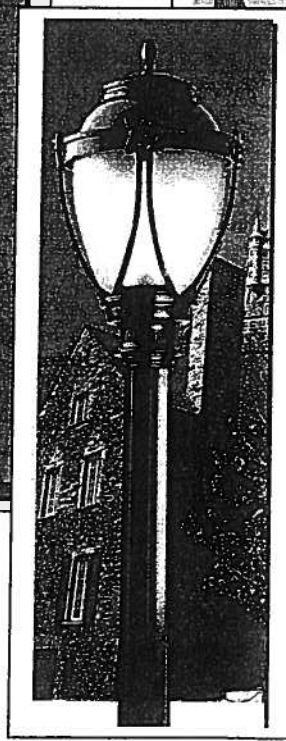
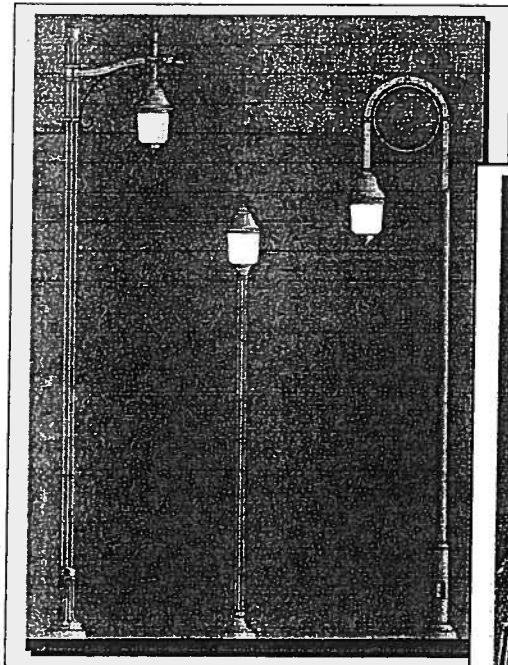
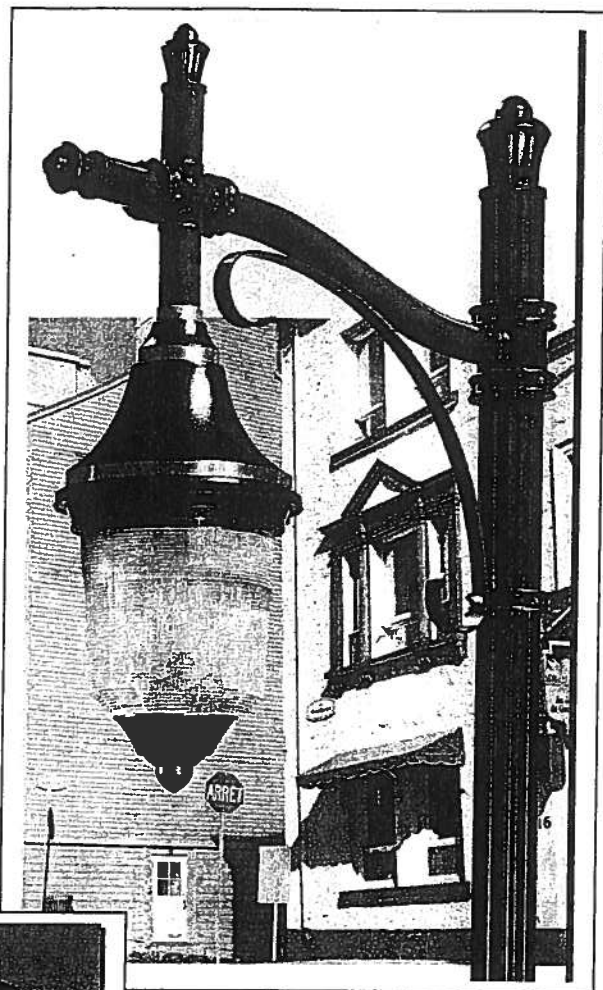
Ornamental Streetlighting

Ornamental streetlighting provides a pedestrian scale to the streetscape and introduces a decorative and sculptural fixture along the curb face.

Current technology has improved the illumination performance of ornamental lighting units to the point where they now rival the performance levels of traditional cobra-head and davit fixtures.

For the Winfield Town Centre project the character of the poles and luminaries will enhance the traditional design concept with the use of decorative poles, bases and luminaries which have the flexibility of being pole top mounted or drop mounted from a decorative arm.

The height of the poles would be approximately 16' and the layout of the poles will be a regular on centre spacing, to satisfy the illumination requirements for the street.



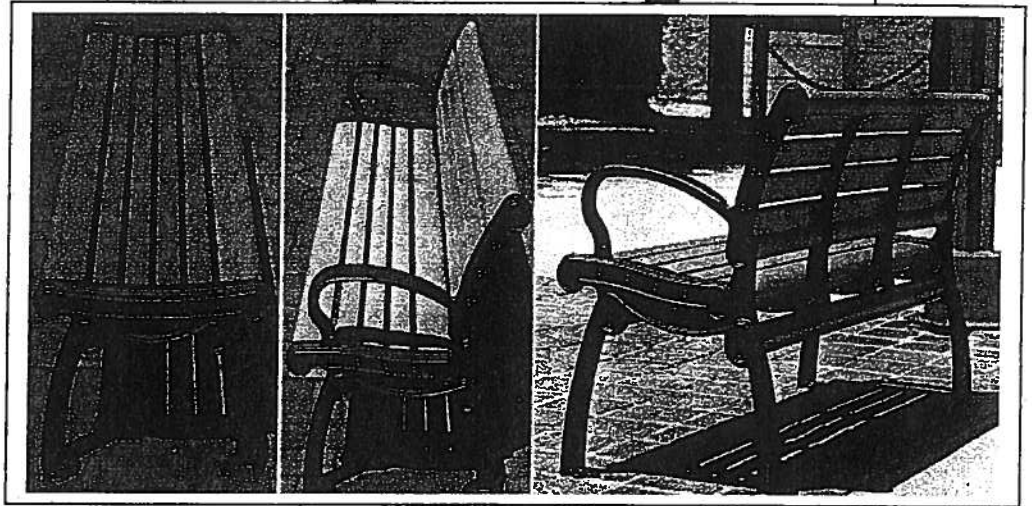
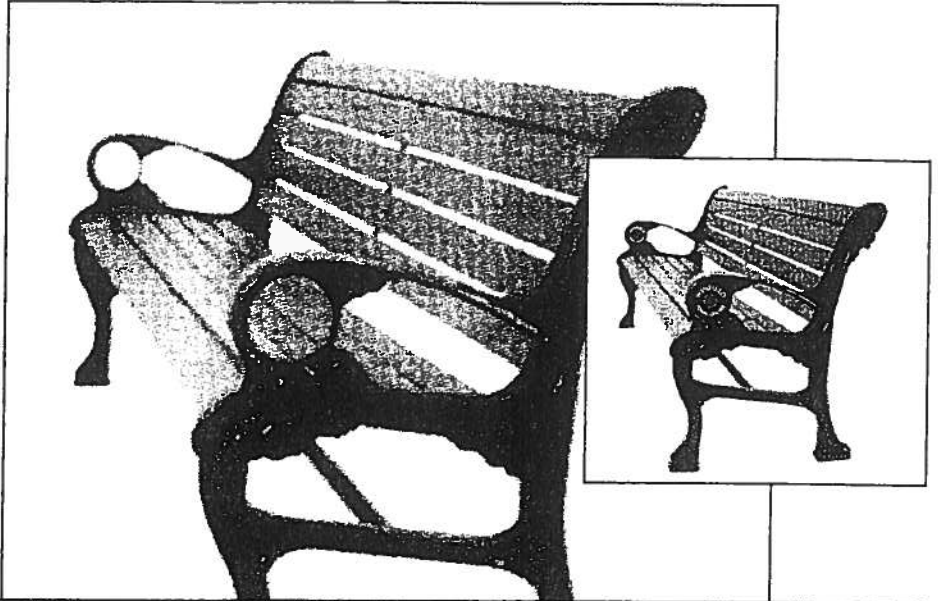
Benches

Benches with cast aluminum or steel frames and wood seating and back slats were recommended as part of the Winfield Town Centre streetscape “kit of parts”.

Given the decorative character of the furniture style, the arm rests and the side frames would easily lend themselves to customization, to individualize these furniture items.

The bench opposite illustrates the possible location for a Town Logo or crest which could be easily incorporated into the casting process.

The Winfield Town Centre Streetscape Committee has also expressed an interest in utilizing a recycled plastic product as a possible material for the street benches in the Town Centre. Styles available in this product are limited and the costs are higher than using wood, but a long term saving may be realized with the reduced vandalism and maintenance costs.

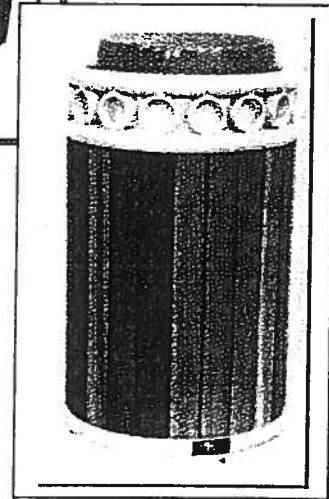
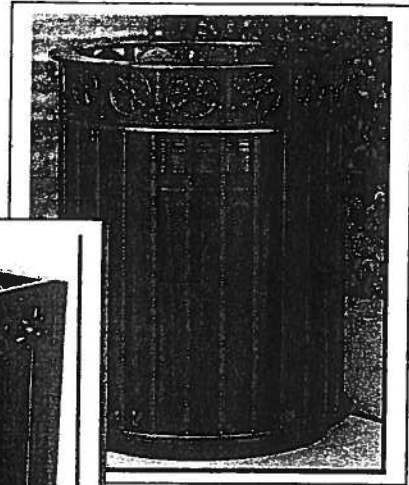
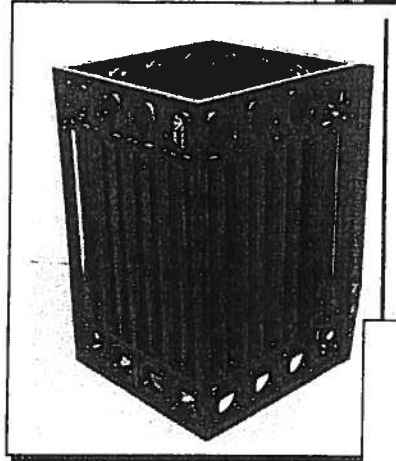


Litter Receptacles

The litter receptacle for the Winfield Town Centre should satisfy the functional requirements of garbage storage, but once again present an opportunity for introducing a sculptural element into the streetscape.

The receptacle should be consistent with the traditional and decorative design direction of the streetscape elements. The detailed ornamentation of the frame will allow for the customization of the receptacle to reflect individual expression and a distinct Winfield character.

The adjacent examples illustrate an open circle pattern in the frame design which may be an ideal location for the Winfield crest or logo and will complement the other family of furniture items on the street.



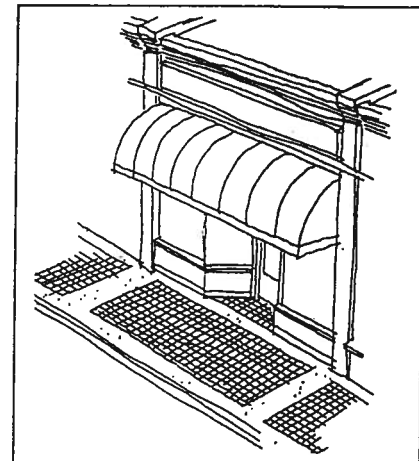
Sidewalk Treatment

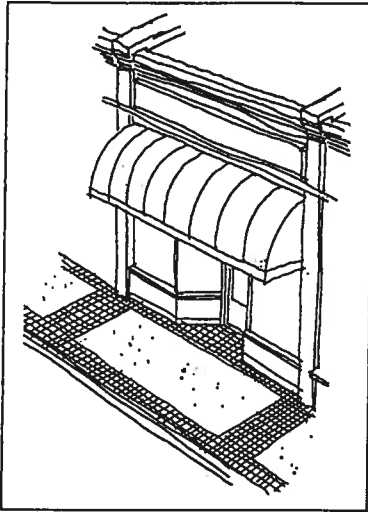
The Winfield Town Centre has been divided into a number of sub areas, which will receive varying levels of streetscape treatment.

The sidewalk treatments will also vary within these sub-areas, with the core area receiving the highest degree of detail.

The sidewalk in the core area will be primarily concrete pavers framed by a band of concrete along both the curb and building face.

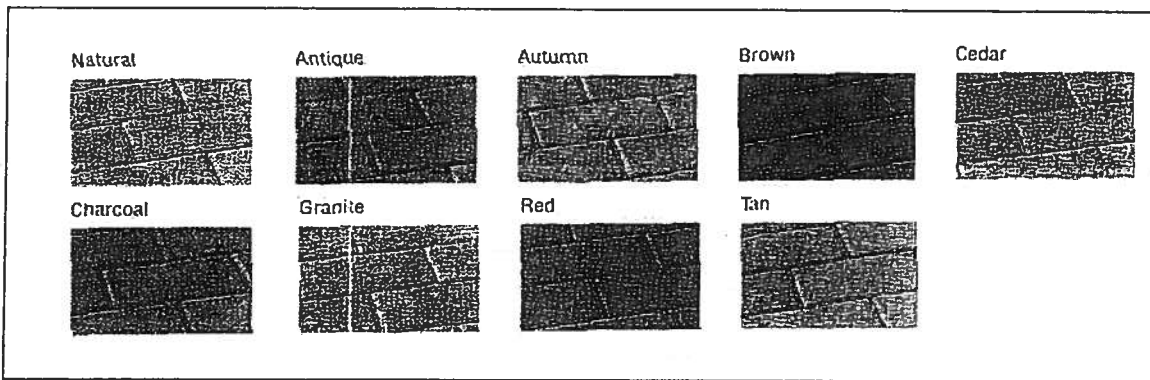
The concrete bands will also frame the pavers perpendicular to the street in a modular pattern which coincides with the parking layout and the placement of the streetlighting.





The sidewalk areas outside of the core will continue with the concept of pavers and concrete, but in these areas the location of the materials will be reversed. Here, the concrete will be the predominant field material and be framed by concrete pavers. The pavers used in the bands will be identical to the pavers used in the core area.

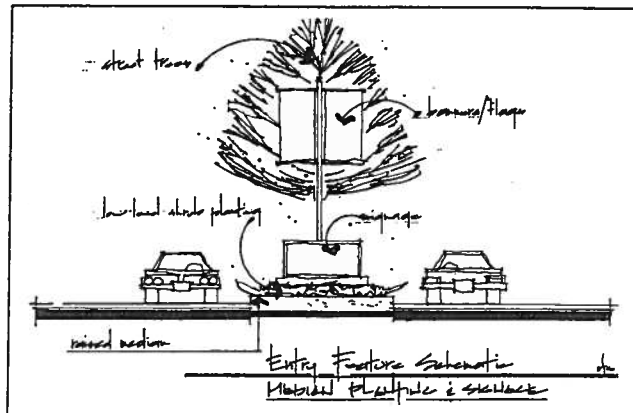
In all areas, the curb flares will be primarily concrete pavers. Stamped concrete has also been suggested as a sidewalk treatment option but preliminary investigation of the costs for this indicate that this may be the most expensive alternative for the sidewalks.



Entry features

The intersection of Main Street with Highway 97 and the intersection of Pollard Road with Highway 97 offer highly visible opportunities to introduce the streetscape character of Main Street to high volumes of vehicular traffic.

The entry features are proposed as an entry or welcome sign located in the centre median, and complemented with tree planting and ornamental shrub massing, and an ornamental streetlight fixture or pole which would display colorful banners or hanging flower baskets.

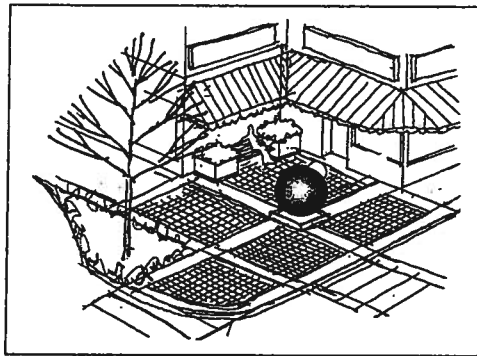


The sidewalk would carry on the streetscape designs with sidewalk trees, ornamental streetlighting and site furniture.

The entry features are an opportunity to introduce the streetscape character of Main Street and present a visual clue to the richness of the streetscape.

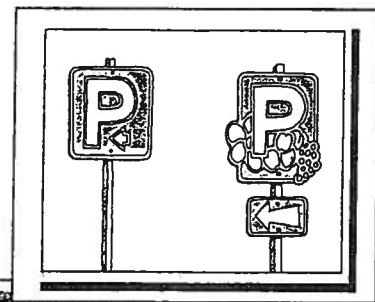
Public Art

Public Art is a key element in the implementation of the streetscape character of the Winfield Town Centre. Opportunities for the display of public art will be incorporated in the sidewalk and curb flares and will initially be concrete pads at grade, which will be transformed into display podiums at such time that sculptural or art pieces become available.



Public art can also be expressed in street banners, painting and wall art, music, crafts, public markets and festivals and drama.

Utilitarian elements (manhole covers, hydrants or regulatory signage) can also be used by artists and sculptors as a canvas on which to display the Winfield crest or logo or the signature elements of Winfield Town Centre.



Street Trees

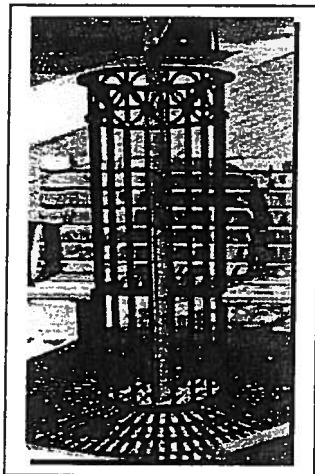
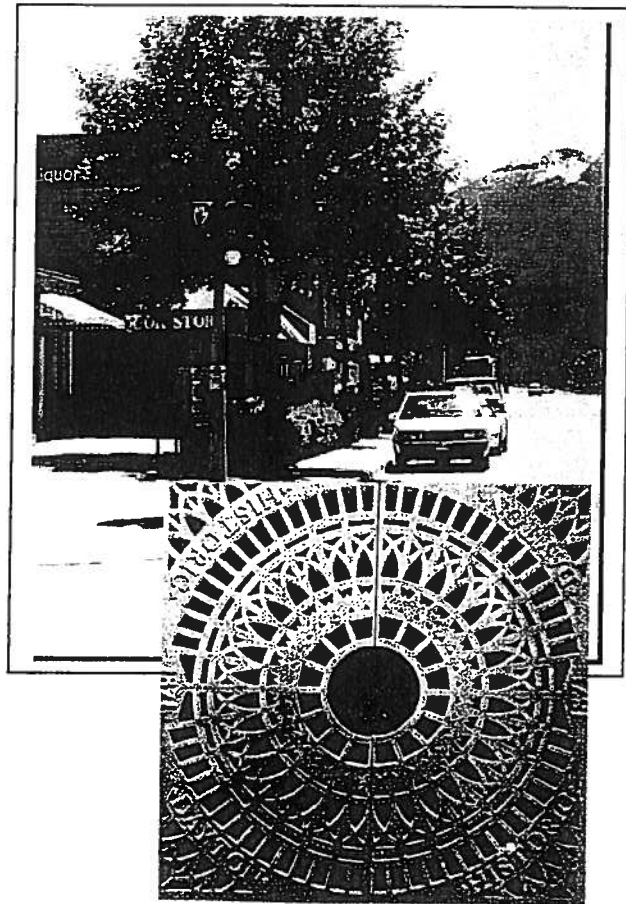
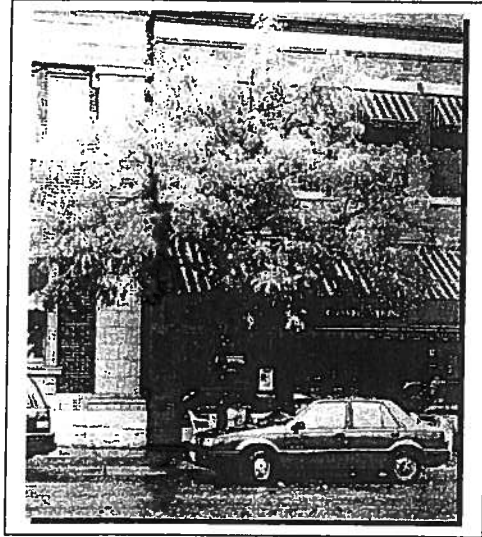
Street trees are the most dominant and significant element of the Winfield Town Centre.

The trees located in the sidewalks and curb flare planting areas will enhance the pedestrian environment with the introduction of color, shade and shadow, and the introduction of a streetscape element which is dynamic....changing with the seasons and the years and maturing as the street evolves.

Street trees will be located in the sidewalk at regular on centre spacing to coincide with the street parking spaces and the streetlighting pattern.

The trees will be located in the sidewalk in tree wells, concrete frames which will inhibit the growth of tree roots under the sidewalk, and support the tree grates which will rest at sidewalk grade.

The trees will be surrounded by above grade a tree guard which will protect the tree in its early years of establishment. The guards also provide another decorative element to the sidewalk and like the grates, offer an opportunity for decorative design and a unique Winfield logo.



A comprehensive pedestrian trail system is proposed in the Town Centre, along the Vernon Creek corridor. The impetus to establish a trail system along this feature has existed for many years. The intentions to develop such a system are documented in the *District of Lake Country Official Community Plan*, the *Winfield Town Centre Concept* (1993) and this document. The principal goals of this initiative are to:

- strengthen the connectivity and accessibility of the Town Centre and area
- enhance opportunities to make innovative use of the Town Centre and the creek
- provide people with the opportunity to experience the natural and educational opportunities of Vernon Creek and its unique landscape

Figure 1 illustrates these goals.

6.1 EXTENT AND FUNCTION

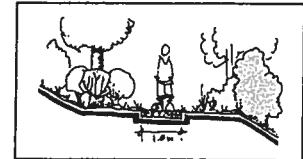
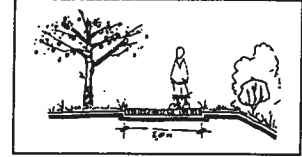
The trail system's principal users will be pedestrians. Cyclists and in-line skaters will have access to paved portions of the trail system where the topography is relatively flat and accessible, and where conflicts between these users and pedestrians are minimal.

The proposed trail system provides two main north-south spines: a creekside route parallel to the east side of the creek and a route between the top of the creek valley's embankment and the future land uses on the east side of Main Street (where there is little distance between Main Street and the top of the embankment the trail system will make use of the sidewalk on the east side of Main Street). There are 5 bridge crossings of the creek proposed along the length of the creek, between Beaver Lake Road and Woods Lake Bottom Road. These provide the opportunity for circle routes within the creek trail system and make strategic linkages between the diversity of uses on either side of the creek. In addition there are trails which provide perpendicular connections between the future streets and land uses of the Town Centre and the two main north-south trails of the creek.

6.2 TRAILS

Two types of trail are proposed:

- An asphalt trail approximately 2.0 metres wide. The majority of trails (i.e., 2,860 m) will be of this type. This trail type will generally be developed on flat and gently-sloped areas. It will provide universal access for pedestrians and access for cyclists and in-line skaters where nearby road and sidewalk routes are not available and where there are minimal conflicts with pedestrians.
- A 1.0 metre wide trail with a surface of crushed aggregate. This material will provide a driveable and permeable surface. A limited amount (i.e., 370 m) of this type of trail will be developed; mainly in steeply-sloped areas and less accessible creekside areas. Use of these trails will generally be limited to pedestrians.

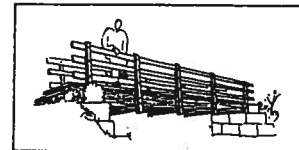


6.3 FACILITIES

In addition to the trails the trail system is comprised of a number of additional facilities or infrastructure that are necessary for the trail system to function properly or that add value to one's experience of the trail system. In general the style of design and construction for these facilities is "rustic" or "wilderness". This treatment will be compatible with the creek setting and more cost effective than "urban" style treatments.

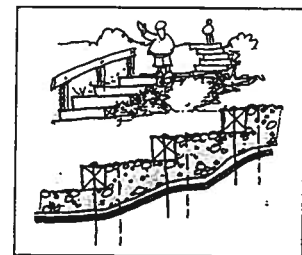
- Bridges

There are 5 proposed pedestrian crossings of Vernon Creek proposed. These are constructed of treated wood decking, railings and spans/beams in a style compatible with the setting. The bridges rest on headwalls of concrete block on opposite banks of the creek.



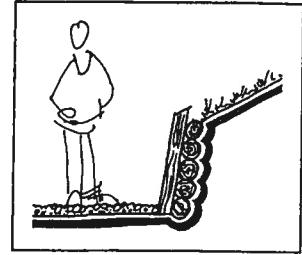
- Steps

It will be necessary to construct steps on some of the steep slopes west of the creek so that pedestrians can have access between the creek and town centre. The steps are informally constructed of wood timbers dug into the existing slope and back-filled with granular aggregate. Wood hand rails are added where required.



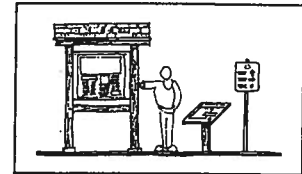
- Retaining Walls

Like steps it may be necessary to construct structures to retain the slope where a trail traverses the steep side slopes on the west side of the creek. These structures, where required, are constructed of treated logs as shown.



- Signs

The signs for the trail system will take on a variety of forms. Their principle function is to advise users of appropriate use and etiquette, provide mapping and direction, and provide interpretive information.



6.4 IMPLEMENTATION

The trail system is a long-range vision. It is recognized that it will take time to acquire access rights and develop the trail system as illustrated in Figure 1 and described above. Therefore implementation of the trail system should be done in phases. The priority for implementation, from highest to lowest, should be as follows:

- Establishment of a north-south trail on the west side of the creek, generally along the top of the slope, from Beaver Lake Road to Woods Lake Bottom Road.
- Connections between this north-south trail and the pedestrian system of Main Street.
- Development of a bridge and creekside trails that connect Main Street with the park/arena/etc. on the east side of the creek.
- Development of other creekside trails (e.g., on the west side of the creek, on the east side of the creek north and south of the existing park/arena site) and the linkages thereto.

In addition to the above phasing priorities it is appropriate to phase development of trails by constructing an aggregate path and not paving it for a while or by installing look-outs, furniture and interpretive signage later.

Costs for the streetscape amenities have been broken up by street sections, similar to the roadworks and servicing, provided under separate cover to the District. The following items have been costed:

- Concrete sidewalk
- Concrete driveways (where necessary)
- Unit pavers
- Concrete landscape edger
- Landscape subgrade
- Topsoil (sodded areas/shrub beds)
- Trees and shrubs
- Nursery sod
- Bark mulch
- Tree well, grate and guard
- Concrete pad and bench
- Concrete pad and bike rack
- Bollards (including concrete foundation)
- Irrigation system complete with electrical
- Streetlights (c/w bases)
- Pavement line painting
- Street signs

(Costs for any public art, special landscape features or land have not been included).

The following cost estimates **do not** include the contingencies and engineering costs nor the GST.

The Vernon Creek Trail System cost estimate includes construction of trails and various features that were discussed in Section 6.

COSTS – STREETScape AMENITIES (MAIN STREET AND POLLARD ROAD)

Section 1	Highway 97 to Beaver Lake Road \$160,220
Section 2	Beaver Lake Road to Hill Road \$199,330
Section 3	Hill Road to Pollard Road \$527,320
Section 4	Highway 97 to Main Street (Pollard Road extension) \$103,835
Section 5	Pollard Road to Mall Property Line \$162,055
Section 6	Winfield Mall Property \$118,950
Section 7	N. Property Line of Mall to Berry Road \$95,275

COSTS – VERNON CREEK TRAIL SYSTEM

\$236,768 (Trails: \$146,286; Features: \$90,500)